



# GRAND PARKWAY H AND I PROJECT QUARTERLY CONSTRUCTION PROGRESS REPORT

Dec 1, 2019 - Feb 29, 2020

April 14, 2020



# HNTB

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Mr. Benjamin H. Asher  
Secretary/Treasurer  
Grand Parkway Transportation Corporation  
125 East 11th Street  
Austin, TX 78701

**Subject: Grand Parkway System – H and I Project**  
**Quarterly Construction Progress Report for Fiscal Quarter Ended February 29, 2020**

April 14, 2020

Dear Mr. Asher,

As the General Engineering Consultant to the Grand Parkway Transportation Corporation (GPTC) and in accordance with Section 407 of the Trust Agreement between GPTC and US Bank National Association, and Section 23 of the TIFIA Loan Agreement with the United States Department of Transportation dated February 21, 2019, HNTB is pleased to submit the Quarterly Construction Progress Report for Grand Parkway Segments H, I-1 and I-2, collectively an additional System Segment, and referred to in this report as the H and I Project. This report covers quarterly construction progress of the H and I Project only through February 29, 2020. Accordingly, the provision of this report does not, under any circumstances, imply that there has been no change in the information presented in this report since February 29, 2020.

As described in the requirements set forth in the GPTC Trust Agreement Section 407, the General Engineering Consultant shall prepare a progress report at least quarterly during the acquisition and construction of any portion of the Grand Parkway System which is financed in whole or in part with Obligations, commencing within 120 days from the initial delivery of the initial Obligations or Obligations financing an additional System Segment. The quarters used for reporting are based on GPTC's fiscal year.

On March 13, 2020, the President of the United States declared the outbreak of COVID-19 a national emergency and the Texas Governor declared it an imminent threat of disaster for all counties (including Chambers, Harris, Liberty and Montgomery) in Texas. Since these declarations were made after the period covered in this report, any impacts from the COVID-19 outbreak on the progress of the H and I Project will be addressed in the next Quarterly Construction Progress Report.

GPTC adheres to the requirements outlined in the Trust Agreement which governs the acquisition or construction of such System Segments financed with Obligations as well as the requirements of the TIFIA Loan Agreement. As required by the TIFIA Loan Agreement Section 23(b)(i), this progress report includes (A) the amount of Total Project Costs expended as of the last date of the quarter as well as during the preceding quarter, and the amount of Total Project Costs estimated to be required to complete the Project (Refer to Section 2.0); (B) availability of funds to complete the Project (Refer to Section 2.0); (C) an assessment of the overall construction progress since the date of the last report, together with an assessment of how such progress compares to the construction schedule (Refer to Section 3.0 through 5.0); (D) most recent projections for the Substantial Completion Date (Refer to Section 4.0); (E) a detailed description of all material problems encountered or anticipated and a detailed description of the proposed solutions (Refer to Section 5.0); (F) delivery status of major equipment (Refer to Section 6.0); (G) proposed or pending change orders (Refer to Section 7.0); (H)

material changes or deviations from the Borrower's land procurement plans or schedule (Refer to Section 8.0).

Additionally, as specified in the Trust Agreement, this report also includes current estimates of: (i) the date on which such System Segment will be opened for traffic, unless such System Segment shall have been opened for traffic prior to such progress report (Refer to Section 4.0), (ii) the Estimated Date of Completion and an Estimated Date of Substantial Completion of such System Segment (Refer to Sections 4.0 and 5.0), (iii) the cost of such System Segment, but excluding any Obligation discount, and the interest during construction and for one year after completion of construction (Refer to Section 2.0), and (iv) the amount of funds required each six (6) months during the remaining estimated period of construction to meet the above described cost of such System Segment exclusive of funds provided for construction contingencies (Refer to Section 2.0), and accompanied by a progress schedule for such construction, and further including, as to construction, comparisons between the actual times elapsed and the actual costs, and the original estimates of such times and costs (Refer to Sections 3.0 through 5.0). This information, as well as other items relevant to each System Segment, is presented in greater detail within this report.

Best regards,

A handwritten signature in blue ink, appearing to read 'SAH', with a long horizontal flourish extending to the right.

Stephen A. Hrncir, P.E.

Program Manager

General Engineering Consultant

## 1.0 INTRODUCTION

This report describes the State Highway 99 Grand Parkway Segment H and I (H and I Project) construction for the second quarter of Fiscal Year 2020, covering the period December 1, 2019 through February 29, 2020. The H and I Project is an extension of the existing Grand Parkway System and is being constructed by the Grand Parkway Transportation Corporation (GPTC). GPTC is a non-profit Texas corporation authorized by the Texas Transportation Commission (Commission) to develop, finance, refinance, design, construct, reconstruct, expand, operate and maintain the Grand Parkway System. GPTC was created under the authority of the Commission and issued bonds to finance development and construction of the System. The project is funded entirely from toll revenue bonds and other obligations issued by GPTC.

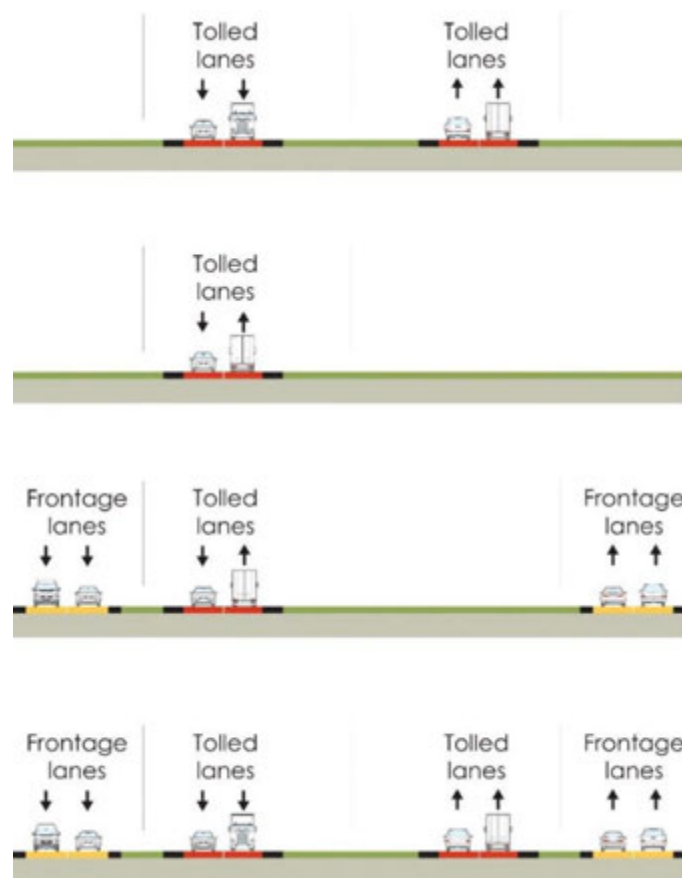
### 1.1 Project Description

The H and I Project is a 52.3-mile section of the overall Grand Parkway Project, a planned 184-mile highway facility around greater Houston extending from State Highway 146 in Galveston County to State Highway 146 in Baytown. It is spread across the seven counties including Brazoria, Chambers, Fort Bend, Galveston, Harris, Liberty and Montgomery. When completed Grand Parkway will provide a third outer loop around the Houston metropolitan area. The Grand Parkway is divided into 11 Segments designated A through I-2 as depicted on the map in Figure 1.1. Segment I-2 is further divided into Segments I-2A and I-2B. The Commission has designated Segments D (Harris County portion), E, F-1, F-2 and G that achieved final acceptance in 2017 and are open to tolled traffic and Segments H, I-1 and I-2 that are under construction as part of the Grand Parkway System.



Figure 1.1: Map of State Highway 99 (Grand Parkway) Project

Segments H and I-1 together are approximately a 37.5-mile long two-lane toll facility (one lane in each direction) with intermittent four-lane sections for passing in Montgomery, Liberty, Harris and Chambers Counties. Segment H includes the H-West Option, which comprises construction of two additional tolled mainlanes for a total of four tolled mainlanes for the first 8 miles from its connection with Interstate Highway 69. Segment I-2B is an approximately 6.1-mile four-lane toll facility in Chambers and Harris Counties. TxDOT is currently constructing improvements to the Segment I-2B non-tolled frontage roads as a separate construction project, which is separate and apart from the H and I Project and is being funded by TxDOT. Lastly, Segment I-2A is an existing 8.7-mile four-lane facility in Chambers County that was constructed by TxDOT in 2008. Segment I-2A will receive tolling equipment upgrades and other improvements as a part of H and I Project. Figure 1.2 shows the typical sections of H and I Project over its length.



**Figure 1.2: Typical Sections of H and I Project**

The development and construction tasks for H and I Project principally include: design development; right of way acquisitions; utility relocations; excavation and embankment; concrete pavement; bridges; retaining walls; drill shafts; rip rap; drainage structures; barricades, signs, and traffic handling; illumination; overhead sign supports; traffic signals; toll facilities; and ITS ducts. H and I Project design, coordination, and construction activities are carried out by a DB Contractor under a Design Build Agreement (DBA) and toll zones are



developed by a Toll System Integrator under a statewide Toll System Integration and Maintenance Agreement.

## **1.2 Design Build Agreement**

On July 31, 2014 TxDOT issued a request for qualifications (RFQ) to design, construct and potentially maintain Segments H, I-1 and I-2 pursuant to a DBA. TxDOT subsequently determined that three of the four teams submitting qualifications statements in response to the RFQ were qualified to be on the short list to submit detailed proposals.

On April 30, 2015 TxDOT issued a request for proposals (RFP) to the short-listed teams. In October 2015, TxDOT received three design build proposals. On January 24, 2017, proposal revisions were received from three short-listed teams following a request for proposal revisions process. The Commission then conditionally awarded the DBA to Grand Parkway Infrastructure (GPI), LLC on March 28, 2017.

Equity member firms for GPI include Ferrovial Agroman (40%), Granite Construction Company (30%) and DBW Construction –Webber (30%).

## **1.3 Project Milestones**

- Conditional Award occurred March 28, 2017
- Contract fully executed on June 30, 2017
- NTP1 issued on July 10, 2017
- Limited NTP2 issued on October 13, 2017
- Option NTP issued for H-West Option on October 27, 2017
- NTP2 (full) issued on October 27, 2017
- GPTC Revenue Bond Financing closed on May 30, 2018
- Start of construction in July 2018
- TIFIA Loan Agreement closed on February 21, 2019
- Substantial Completion to occur within 1,693 days after NTP1
- Substantial Completion Deadline is February 27, 2022
- Projected Substantial Completion date is May 12, 2022
- Projected Open to Traffic date is May 12, 2022
- Assumed Revenue Commencement date per GPTC System T&R forecast is May 22, 2022
- Final Acceptance to occur 120 days after Substantial Completion
- Final Acceptance Deadline is June 27, 2022
- Projected date of Final Acceptance is September 9, 2022

## **1.4 Purpose of the Report**

GPTC Trust Agreement Section 407 states that the General Engineering Consultant shall prepare a progress report at least quarterly during the acquisition and construction of any portion of the Grand Parkway System which is financed in whole or in part with Obligations, commencing within 120 days from the delivery of the initial Obligations or Obligations financing an additional System Segment. The H and I Project is considered an additional System Segment to the Grand Parkway System as defined by the GPTC Trust Agreement. Furthermore, Section 23(b)(i) of the

TIFIA Loan Agreement requires that the GPTC furnish the TIFIA Lender a Quarterly Construction Progress Report at such time as required by the Trust Agreement to be filed with the Trustee .

This Quarterly Construction Progress Report complies with the reporting requirements of Section 407 of the Trust Agreement as well as the TIFIA Loan Agreement. This Quarterly Construction Progress Report provides the required information as of last calendar date of the GPTC Fiscal Year quarter preceding its submittal.

## 2.0 Project Costs

Table 2.1A provides: (a) a detailed breakdown of the GPTC bond proceeds available for H and I Project, (b) an adjusted budget showing an estimate of total project costs at completion, (c) actual costs-to-date as of previous quarter end, or November 2019, (d) actual costs-to-date as of end of current quarter, or February 2020, (e) total actual costs-to-date expended as of February 2020 and (f) a forecast comprising an estimate of costs-to-complete. Table 2.1B provides: (a) a detailed breakdown of the TxDOT budgets to cover portion of TxDOT's Agency Costs and Segment I-2B Frontage Road construction costs, (b) an adjusted budget showing an estimate of total project costs at completion, (c) actual costs-to-date as of previous quarter end, or November 2019, (d) actual costs-to-date as of end of current quarter, or February 2020, (e) total actual costs-to-date expended as of February 2020 and (f) a forecast comprising an estimate of costs-to-complete. Table 2.2 provides a forecast of cost for each six-month period over the remaining duration of construction and extends one-year beyond the completion of construction date to ensure all costs are captured.

Actual costs paid-to-date and an estimate of funds required for processing of DB Contractor's draw for the remaining duration of construction are shown in Table 2.3 on a semiannual and cumulative basis. Estimate of funds required is based on the approved maximum payment schedule, including the H-West Option from the DBA, and approved change orders. Approved maximum payment curve is indicative of the DB Contractor's original estimates of costs over time at the time of the contract award, with subsequent adjustments for approved change orders. The DB Contractor's reported earned value for the work completed is as of the latest approved draw request received for the quarter. Cumulative earned value reported as of the most recent quarter is below the maximum payment indicating that the DB Contractor's progress is not constrained from cash flows. Refer to Section 5 of this report for a discussion on schedule progress against the latest approved baseline schedule. Paid-to-date represents the amount that has been disbursed and trails the cumulative earned value estimate of work completed and constraints (if any) from the maximum payment schedule included in the DBA. Difference in earned value and paid-to-date amounts also accounts for the time it takes for DB Contractor to assemble and submit the draw request after the cut-off date, address TxDOT review comments, resubmit as necessary and for TxDOT to approve and process the payment.

The H and I Project is currently expected to be completed within the total budgeted amount shown in this report, which includes a contingency amount. The contingency amount is anticipated to be adequate to address increases in project costs relating to change orders and other cost uncertainties.

**Table 2.1A: GPTC H and I Project Budgets, Actuals, Forecasts and Total Estimated Costs (\$ Thousands)**  
**(Paid by Bond Proceeds)**

	GPTC Bond Proceeds Budget <sup>5</sup>			Actuals <sup>5</sup>			Forecast <sup>4</sup>	Estimate at Completion (Actuals and Forecast)
Element	Original Proceeds	Adjustments Thru Feb'20	Adjusted Total	Expended Thru Nov' 19	Expended This Quarter	Expended Thru Feb'20	Estimate to Complete	
Segments H, I-1, & I-2								
Design-Build Agreement <sup>1,3</sup>	894,702	12,245	906,946	247,010	64,265	311,275	595,672	906,946
Right of Way Acquisition <sup>1</sup>	330,000	-	330,000	99,159	18,505	117,664	212,336	330,000
Toll Equipment & Integration <sup>1</sup>	32,000	6,894	38,894	864	191	1,054	37,840	38,894
Environmental Mitigation <sup>1</sup>	24,000	(12,000)	12,000	9,948	1	9,949	2,051	12,000
TxDOT Agency Costs <sup>1</sup>	53,737	7	53,744	27,489	2,609	30,098	23,646	53,744
Project Contingencies <sup>1</sup>	110,000	(7,146)	102,854	-	-	-	102,854	102,854
Subtotal Project Costs	\$1,444,438	-	\$1,444,438	384,469	85,570	470,040	974,399	\$1,444,438
Financing Costs								
Capitalized Interest (2018 Bonds/BANs) <sup>1</sup>	309,855	-	309,855	99,137	-	99,137	210,718	309,855
Cost of Issuance (2018 Bonds/BANs) <sup>1</sup>	11,537	-	11,537	11,306	4	11,310	227	11,537
Capitalized Interest (50% of Series 2013E) <sup>2</sup>	29,639	-	29,639	29,639	-	29,639	-	29,639
Cost of Issuance (50% of Series 2013E) <sup>2</sup>	958	-	958	958	-	958	-	958
Rate Stabilization Fund <sup>1</sup>	50,000	-	50,000	50,000	-	50,000	-	50,000
Subtotal Financing Costs	\$401,988	-	\$401,988	\$191,039	\$4	\$191,043	\$210,945	\$401,988
Total Project and Financing Costs	\$1,846,427	-	\$1,846,427	\$575,508	\$85,574	\$661,083	\$1,185,343	\$1,846,427

Figures shown may not add to totals due to rounding.

**Notes:**

1. Reflects information provided in the GPTC Annual FY 2019 Capital Construction Budget ("FY19 Budget") for Segments H&I, as adopted by the board on August 21, 2018, including budget adjustments, actual costs and forecasted costs, updated to reflect results of the reporting period.
2. Reflects 50% of capitalized interest and cost of issuance for the Series 2013E Bonds.
3. Design Build Agreement Adjustments reflect only change orders that have been approved.
4. H&I Forecast – see Table 2.2 for allocation of forecast into six-month periods.
5. Table shows budgeted amounts and actual expenditures of the GPTC for the H and I project. It does not include expenditures by TxDOT for procurement, feasibility studies and District's frontage road project in Segment I-2B supported through other sources – see Table 2.1B for details.

**Table 2.1B: TxDOT H and I Budgets, Actuals, Forecasts and Total Estimated Costs (\$ Thousands)**  
**(Paid by Fund 0006)**

	TxDOT Budget <sup>1</sup>			Actuals <sup>1</sup>			Forecast	Estimate at Completion (Actuals and Forecast)
Element	Original Budget	Adjustments Thru Feb'20	Adjusted Total	Expended Thru Nov'19	Expended This Quarter	Expended Thru Feb'20	Estimate to Complete	
TxDOT Agency Costs								
TxDOT Management	1,885	-	1,885	1,885	-	1,885	-	1,885
Feasibility Studies	8,840	-	8,840	8,840	-	8,840	-	8,840
Traffic and Revenue Studies	1,126	(3)	1,123	1,123	-	1,123	-	1,123
Procurement, Legal and Financial	6,660	-	6,660	6,660	-	6,660	-	6,660
Subtotal TxDOT Agency Costs	\$18,511	(\$3)	\$18,508	\$18,508		\$18,508		\$18,508
TxDOT District Project								
District I-2B Construction Project <sup>2</sup>	59,000	2,193	61,193	61,422	871	62,293	38	62,331
Subtotal TxDOT District Project	\$59,000	\$2,193	\$61,193	\$61,422	\$871	\$62,293	\$38	\$62,331
Total Project and Financing Costs	\$77,511	\$2,190	\$79,701	\$79,931	\$871	\$80,802	\$38	\$80,839

Figures shown may not add to totals due to rounding.

**Notes:**

1. Table shows budgeted amounts and actual expenditures incurred by TxDOT under Fund 6. It does not include expenditures supported through GPTC bond proceeds – see Table 2.1A for costs supported by GPTC bond proceeds.
2. District I-2B Frontage Road Construction Project is Substantially Complete with Final Acceptance pending completion of punch list items. Budget adjustment to match actual costs at completion is pending closeout activities and will be reported at that time.

**Table 2.2: H and I Project Forecast Costs (\$ Thousands)**

Element	Forecast <sup>2</sup>						Total Estimate to Complete	
	FY 2020	Fiscal Year 2021			Fiscal Year 2022			FY 2023
	6-month	6-month	6-month	6-month	6-month			
<b>Segments H, I-1, &amp; I-2</b>								
Design-Build Agreement Costs <sup>1</sup>	130,996	136,407	123,217	116,479	88,573	-	595,672	
Right-of-Way Acquisition	97,204	54,454	54,454	2,750	2,750	724	212,336	
Toll Equipment & Integration	6,572	8,823	8,823	6,812	6,812	-	37,840	
Environmental Mitigation	925	543	543	20	20	-	2,051	
TxDOT Agency Costs	7,057	4,881	4,881	3,413	3,413	-	23,646	
Project Contingencies	-	-	-	-	-	102,854	102,854	
<b>Subtotal Project Costs</b>	<b>\$242,754</b>	<b>\$205,108</b>	<b>\$191,918</b>	<b>\$129,473</b>	<b>\$101,567</b>	<b>\$103,578</b>	<b>\$974,399</b>	
<b>Financing Costs</b>								
Capitalized Interest (2018 Bonds/BANs)	37,099	37,099	37,099	37,099	37,099	25,222	210,718	
Cost of Issuance (2018 Bonds/BANs)	227	-	-	-	-	-	227	
Capitalized Interest (50% of Series 2013E)	-	-	-	-	-	-	-	
Cost of Issuance (50% of Series 2013E)	-	-	-	-	-	-	-	
Rate Stabilization Fund	-	-	-	-	-	-	-	
<b>Subtotal Financing Costs</b>	<b>\$37,326</b>	<b>\$37,099</b>	<b>\$37,099</b>	<b>\$37,099</b>	<b>\$37,099</b>	<b>\$25,222</b>	<b>\$210,945</b>	
<b>Total Project and Financing Costs</b>	<b>\$280,080</b>	<b>\$242,207</b>	<b>\$229,017</b>	<b>\$166,572</b>	<b>\$138,666</b>	<b>\$128,800</b>	<b>\$1,185,343</b>	

Figures shown may not add to totals due to rounding.

**Notes:**

1. Design Build Agreement Adjustments reflect only change orders that have been approved.
2. Does not include TxDOT Fund 6 forecast for District I-2B Construction Project shown in Table 2.1B.

**Table 2.3: H and I Project Design Build Agreement Construction Draw (\$ Thousands)**

Period		Estimate of Construction Draw <sup>1,4</sup>		Earned Value <sup>2</sup>		Cumulative Paid-to-Date <sup>3</sup>
Begin	End	Period	Cumulative	Period	Cumulative	
NTP1	8/31/2017	12,403	12,403	12,403	12,403	12,403
9/1/2017	2/28/2018	25,125	37,528	40,711	53,114	37,528
3/1/2018	8/31/2018	42,806	80,334	32,605	85,719	80,334
9/1/2018	2/28/2019	140,566	220,900	92,455	178,174	178,174
3/1/2019	8/31/2019	209,027	429,927	68,836	247,010	247,010
9/1/2019	2/29/2020	163,629	593,556	106,106	353,116	311,275
3/1/2020	8/31/2020	110,101	703,657			
9/1/2020	2/28/2021	127,674	831,331			
3/1/2021	8/31/2021	59,155	890,486			
9/1/2021	2/28/2022	16,460	906,946			

**Notes:**

1. Estimate of Construction Draw is based on the maximum payment schedule, including the H-West Option, and executed change orders.
2. Earned Value is based on construction work completed per the latest draw request as of the reporting period.
3. Paid-to-Date represents amounts that were disbursed as of this quarter end but are reported in the periods they were earned.
4. Estimate of Construction Draw reflects only change orders that have been approved.

**3.0 Overall Progress of Construction**

Construction activities started on the project in summer of 2018 and have continued to progress. Additionally, DB Contractor progressed its debris removal, cleanup and site restoration activities, including repairs to works damaged by Tropical Storm Imelda through the quarter. See Section 5 of this report for additional information on schedule impacts from Tropical Storm Imelda. Overall, design submittals have been made to TxDOT for review and comment, leading to release-for-construction documents along the corridor. The DB Contractor continued to progress environmental coordination, permitting, right-of-way, utilities and public outreach activities. A summary of progress for the quarter is summarized below and illustrated in construction photographs included with Appendix A.

**3.1 Public Information & Communications by DB Contractor**

Domain name [www.sh99grandpkwy.com](http://www.sh99grandpkwy.com) is maintained by the DB Contractor and continues to be a source of information for the project. Additionally, the DB Contractor is required to maintain a Public Information Office located at 7560 FM 1960, Dayton to serve walk-in customers.



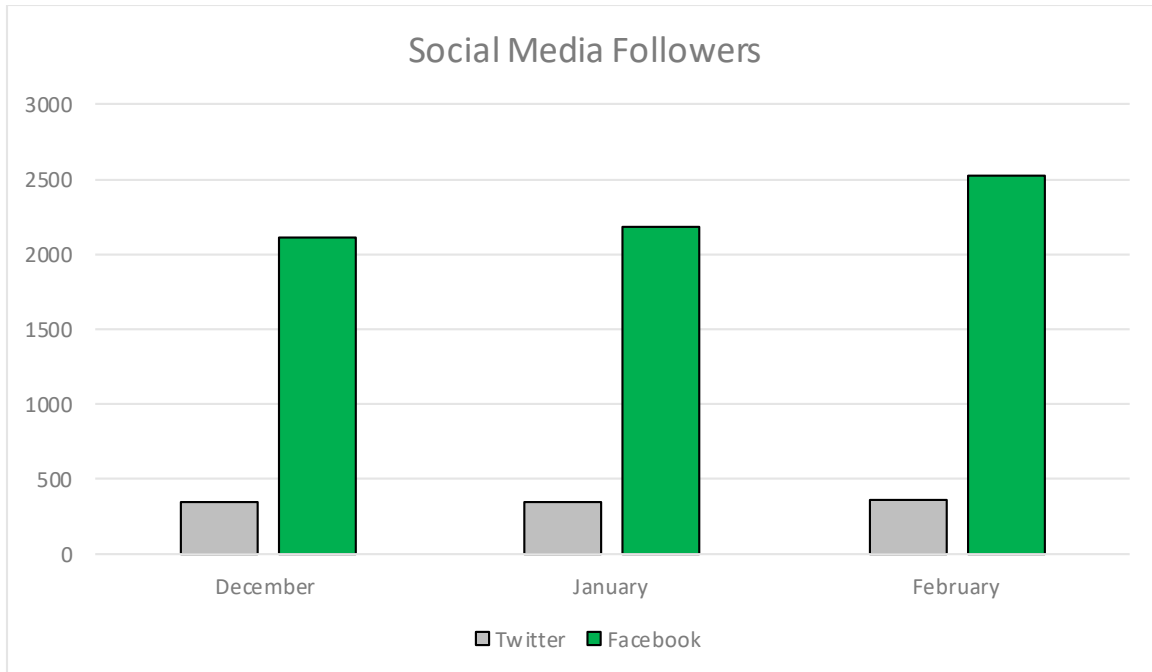
**Figure 3.1: Grand Parkway Public Information Center**

Contact subscription list for the project stakeholders continues to grow. At closing of this quarter, 1,062 recipients (elected officials, TxDOT, corridor residents, business owners and GPI staff) are signed up to receive project alerts, newsletters, etc.

Social media presence for the project continued to increase. At closing of this quarter, the project website had 363 followers on Twitter site and 2,519 followers on Facebook. The number of people who had viewed any posts from the GPI Facebook page on their screen was 37,486 for the most recent month of the quarter ended February 2020. Figure 3.2 shows a snapshot of presence on social media.

DB Contractor participated in various coordination meetings, conducted outreach and made presentations at networking events regarding the project to various interested parties in this quarter. This included:

- Quarterly meeting with Chambers County
- City of Dayton Officials
- City of Mont Belvieu Officials
- Liberty County Officials
- Missouri Street Church of Christ in Baytown
- Baytown Police Department and Jail Break Run organizers in Baytown



**Figure 3.2: Grand Parkway Social Media Presence**

### 3.2 Utilities

DB Contractor continues to negotiate PUAA's with utility owners and has completed sending out the PUAA's to all known affected utility owners. Twenty five (25) PUAA's have been fully executed. The remaining PUAA's are currently being reviewed by the utility owners whereas Phoenix Hydrocarbon & SHECO PUAA's are being negotiated. Overall status of the utility coordination is shown in the following table.

**Table 3.1: Status of Utilities Work**

Utility Status	
Anticipated Total Utility Adjustments	343
<b>PUAA's</b>	
Anticipated Total	39
Executed	25
Remaining	14
<b>UAAA's</b>	
Anticipated Total	63
Executed	31
Remaining	32

Anticipated total utility adjustments represents an estimate of conflict locations requiring adjustment based on the DB Contractor's design progress. As the roadway design progresses,



conflicts may be avoided, or additional conflicts may be identified. The DB Contractor continues to progress pending PUAA's, UAAA's and design for the remaining Owners. Additionally, the DB Contractor is verifying civil design impacts and working on completing AUA's with Utility Owners for utilities that are not in conflict and its right of way team is currently pursuing the required easement documents.

Status of DB Contractor managed design is as follows:

- Undine – 100% plans being reviewed for approval.
- Mt. Belvieu – 100% design approved by the utility owner.
- DECON – Protective slab design is in progress.
- AT&T – 100% of relocations addressed and being designed.

Status of utility owner managed design is as follows:

- Entergy Transmission – Designs are complete and pending TxDOT submission.
- Entergy Distribution – Designs along FM 1960, FM 1413, & US 90 are complete and now designing priority areas along FM 1485.
- CenterPoint Electric – DB Contractor has completed reviews of CenterPoint provided draft design and CenterPoint is compiling agreements.
- Comcast – Provided DB Contractor with record/reference drawings with 80% of the outstanding designs provided by Comcast.

Utility coordination, design and field adjustment work is moving forward at a steady pace and the DB Contractor continues to make progress according to the schedule.

### **3.3 Permits/Environmental**

NEPA reevaluation documentation submitted for TxDOT review includes the following:

- Additional ROW at CR 491 – For Drainage
- Additional ROW N of US 90 – For Drainage
- Additional ROW for Ponds – Parcel 1156
- Additional ROW for Embankment Areas – Multiple Locations in Seg H2 and I1A
- Drainage Easement at Station 2100+00
- Corner Clips at FM 565
- Gum Grove Road Drainage Easement
- Revised Drainage Outfall into Hackberry Gully @ Langston Blvd

In Segment I2, NEPA reevaluation for Wismer Road is completed and ready for submittal for TxDOT review.

Asbestos containing material & lead based paint abatement activities include:

- Lead Based Paint on Cedar Bayou RR Bridge: Abatement for the bridge removal has been completed.
- Phase II activities at Parcel 862 and 863 have been completed and DB Contractor is gathering cost information for Phase II at Parcel 1311 (Placid Tank Farm)

- Structures Surveys –Parcels that have structures are continually being surveyed for Asbestos Containing Materials (ACM) and Lead Based Paint (LBP), with abatement scheduled on an ongoing basis.

For potential borrow source locations, DB Contractor continues to work on the necessary environmental field studies to satisfy Special Condition 2 and 4 of the USACE Individual Permit (IP) and Nation-Wide Permit (NWP). The DB Contractor will revise IP once all design changes have been completed.

Coordination with USACE is ongoing, a meeting with the USACE was held in December regarding the modification to the IP.

The DB Contractor continues to conduct continuous monitoring and protection of nesting sites, in compliance with the Migratory Bird Treaty Act, in all active construction locations. DB Contractor resumed this monitoring in February and will continue through September for the year.

### **3.4 Right of Way**

In accordance with the terms of the DBA the DB Contractor is responsible for right of way surveys, appraisals, environmental phase I assessments, acquisition packages, title searches and relocation packages. Overall the schedule of right of way acquisitions and associated costs are tracking against the DB Contractor's baseline schedule. The following table provides a detailed status of progress being made towards releasing parcels for construction through this reporting period. It is noted that the table individually accounts for parcels that sometimes are combined into a single parcel or are eliminated after ROW survey is completed because they did not reflect an affected parcel. An assessment of actual parcels acquired and released for construction against total to be acquired is discussed in Section 8 of this report. HNTB and Atkins, TxDOT Houston District's ROW acquisition management consultant, are monitoring the critical path for right of way parcels in condemnation.

**Table 3.2: Status of Right of Way Work**

Item	Activities Completed Previous Periods	Activities Completed Current Period	Total Completed Activities	Activities In-Progress
Pre-Acquisition (Title Searches, ROEs, etc.)	316	0	316	0
Appraisals (prep, approval for initial and ED update)	534	11	545	2
Surveys (prep, review, approval)	468	0	468	0
ESA Phase I's	244	0	244	0
Acquisition Packages (prep, review, approval)	463	0	463	1
Initial Offers Made	231	0	231	0
Final Offers Made	231	0	231	0
Closing Payment Packages (prep, approval, funding for PUA's and signed deeds)	434	13	447	0
Closings	137	8	145	3
Relocation, Relocation Payment Packages and 90-Day NTV's	217	5	222	0
ED Packages (prep, review, approval)	125	9	134	1
ED Proceedings (AAG approval and setting hearing)	86	15	101	20
ED Payment Packages (prep, approval, funding, deposit and 30-day NTV)	221	29	250	2
Released for Construction	180	25	205	0

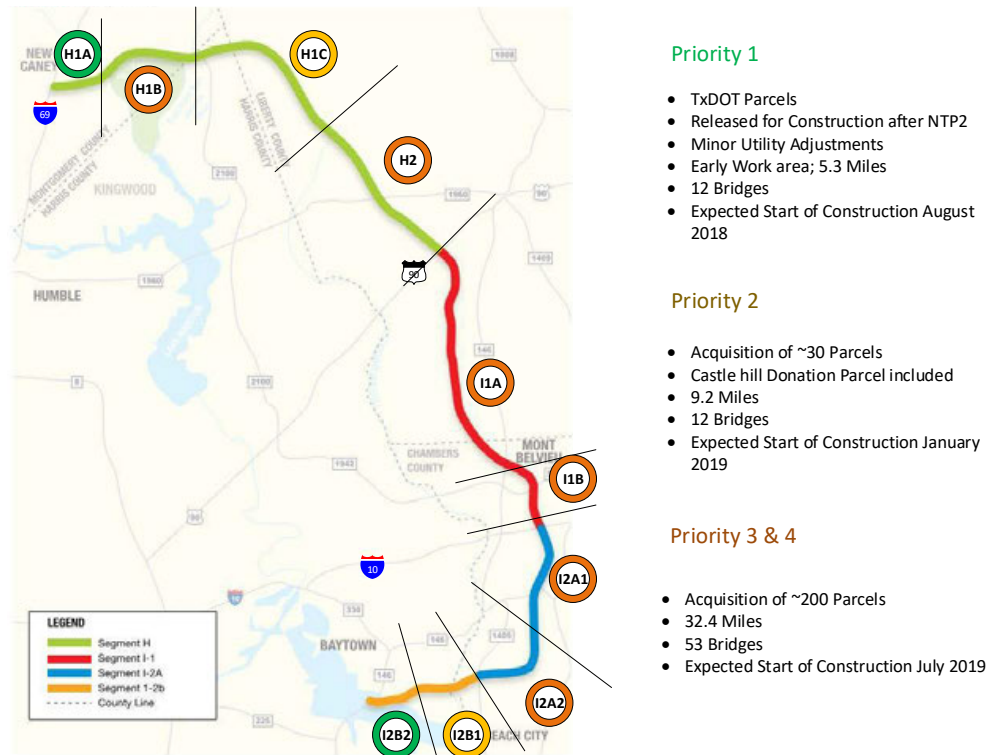
Note: For a discussion of parcels released into construction against total number of parcels to be acquired, see Section 8 of this report

### 3.5 Design

DB Contractor is designing the project in priority areas as depicted in Figure 3.3. These areas represent the priority for construction based on availability of right of way. The subsegments that have top priority are marked with green circles and consist of Section H1A; US-69 to East of LP 494 and Section I2B2; SH 146 to Cedar Bayou. The subsequent priorities by subsegments are shown in yellow and orange respectively.

The progress up to end of the current quarter is as follows:

- Design has been progressing and is approximately 98% complete.



**Figure 3.3: DB Contractor Priority Areas for Design Development**

- DB Contractor continues to review and develop the remaining design, both in terms of technical compliance and value-engineering opportunities.
- Regular DB Contractor meetings with the design consultants were ongoing to progress the design submittals.
- DB Contractor continues to coordinate design development with TxDOT through meetings and RFI submittals.
- Released-for-Construction submittals have been made for Segments H1A, H1B, H1C, H2, I1A, I1B, I2A1, I2A2, I2B1 and I2B2.
- GEC is regularly tracking notice of design changes and any revisions to the released drawings.
- DB Contractor continues to respond to TxDOT submittal comments.

### 3.6 Tolling Systems

Coordination is on-going between the DB Contractor and TxDOT Toll Operations Division for design at tolling locations. TxDOT Toll Operations Division with support from design consultant Atkins and System Integrator, TransCore, is responsible for design and construction of tolling infrastructure in the toll zones. According to the DBA, the DB Contractor is required to complete work in toll zones 180 days prior to Substantial Completion Deadline and handover to the Toll System Integrator.

### 3.7 Quality

DB Contractor has mobilized its quality control staff as well as Independent Quality Firm staff to perform inspection and testing. Construction quality control and hold point inspections for construction were performed. DB Contractor has instituted a process for reporting non-conforming work and is maintaining a log of non-conformance reports and of Construction Deficiencies. Status of quality related actions as of the closing of this quarter:

- Total NCRs issued as of closing of current quarter: 112 (36 open)
- Observation Reports issued as of closing of current quarter: 16 (1 open)
- Engineering Judgements: 89
- Construction Deficiencies: 101 (11 open)

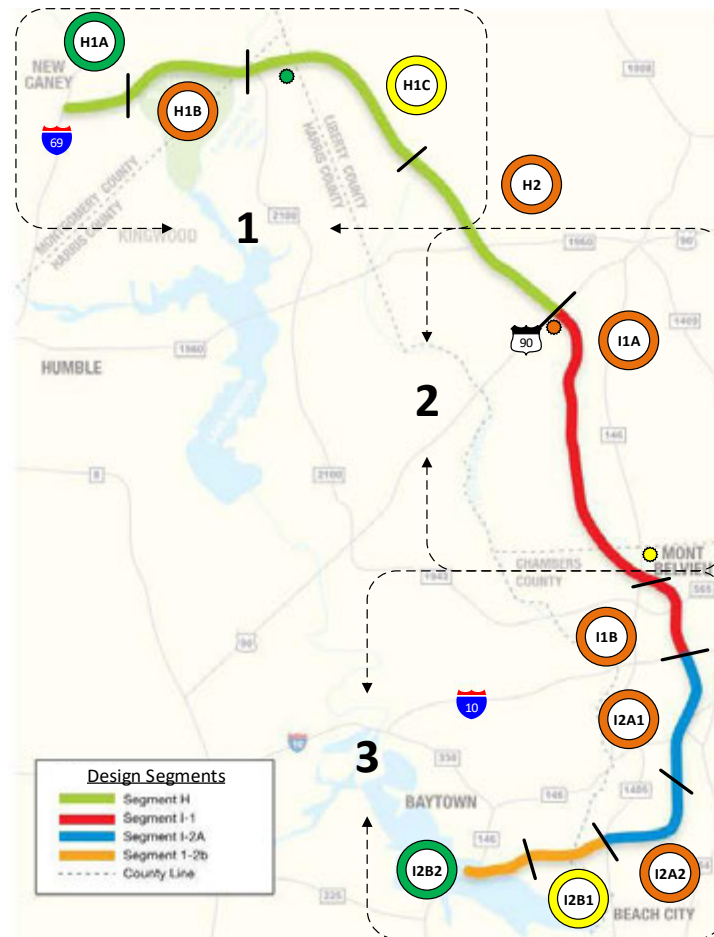
### 3.8 Maintenance During Construction

With the issuance of full NTP2, the DB Contractor is responsible for maintaining all existing roadways within the project right of way of Segments H and I-1 and is responsible for elements modified or constructed by the DB Contractor in Segment I-2. The DB Contractor has developed procedures under its Maintenance Management Plan to inspect and repair facilities on an ongoing basis. Debris removal and sign straightening have been the main items of maintenance work in this quarter. Activities carried out by the DB Contractor include:

- Weekly Inspections Completed: 9
- Monthly Inspections Completed: 3
- Special Inspections Completed: 0
- Incident Responses Attended: 0
- Defects Identified this Period: 8
- Defects Resolved this Period: 8
- Defects Total to Date: 71
- Defects Resolved to Date: 67
- Defects Currently Unresolved: 4

### 3.9 Construction

The DB Contractor is constructing the project in three segments. Figure 3.4 shows the correlation of design priority subsegments to the three construction segments.



**Figure 3.4: Construction Segments and Priority Design Areas**

## Construction Segments

### Construction Segment 1:

- Segment Manager: Jason Pittenger (Webber)
- From Segment G Tie-In near US 59 to North of FM 1960 (17.1 Miles)
- 45 Bridges with 871k SF of Deck; 14k CY Substructure
- 1.35M CY EX and 3.3M CY EMB; 1.9M CY to Borrow

### Construction Segment 2:

- Segment Manager: Brais Saurez (Ferroval)
- From North of FM 1960 to SH 146
- STA 1920+00 to STA 2812+00 (16.5 Miles)
- 19 Bridges with 523k SF of Deck; 10k CY Substructure (includes a 4,000+ LF Bridge over Railroad)
- 602k CY EX and 3.5M CY EMB; 2.9M CY to Borrow
- Segment will also oversee work in Luce Bayou

### Construction Segment 3:

- Segment Manager: Chris Allen (Granite)
- From SH 146 to Southern Terminus of the Project
- STA 2812+00 to STA 2141+19 (6.2 Miles)
- STA 445+31 to STA 617+93 (1.4 Miles)
- STA 74+00 to STA 351+48 (5.3 Miles)
- 25 Bridges with 235k SF of Deck; 4.5k CY Substructure
- 400k CY EX and 3.2 CY EMB; 2.8M CY to Borrow
- Segment will oversee the Cedar Bayou Lift Bridge

For the reporting period, the DB Contractor construction operations in each of the three construction segments broken out by priority area where applicable is as follows:

### Segment 1:

Activities in H1A included:

- Continued to establish embankment and proof rolling from US 59 to Loop 494
- Levelled up, installed coping and flumes for Retaining Walls 6B, 7 and 8
- Formed and poured flumes for Retaining Walls 3 and 4
- Constructed abutment 21
- Progressed cement treatment west of US 59 and from US 59 to Loop 494
- Installed reinforced concrete box using jack and bore method east of US 59
- Continued with bond breaker from US 59 to Loop 494
- Carried out organics and sulfates sampling

- Progressed bridge deck for units 1 & 2 and completed deck panels for at bents 1 through 12 for Bridges 203 and 204
- Completed Abutment 1 approach slab for Bridges 203 and 204
- Formed, poured and stripped bent caps at bents 13 and 14 for Bridges 203 and 204
- Delivered and tied rebar cages for bent caps and columns, and formed poured and stripped columns caps at bent 15 through 17 for Bridges 203 and 204
- Completed setting beams for Bridges 203, 204 and 206

Activities in H1B included:

- Carried out organics and sulfates sampling
- Performed ROW Inspections
- Carried out tree cutting and grubbing activities
- Continued to establish embankment utilizing day and night shifts
- Worked on MSE panels and backfill for Retaining Walls 1, 2 and 3
- Levelled up, installed coping and flumes for Retaining Walls 1, 2 and 3
- Levelled up and completed foundation improvements at Retaining Walls 4, 5 and 6
- Continued proof rolling, including construction of access and drainage ditches
- Delivered and tied rebar cages for drilled shafts, and drilled and poured drilled shaft foundations for Bridges 209 and 210
- Formed and poured drilled shaft extensions for Bridges 213 and 214
- Delivered and tied rebar cages for drilled shafts and columns for Bridges 217 and 219

Activities in H1C included:

- Carried out organics and sulfates sampling
- Continued slope paving and rip rap placement at Luce Bayou Siphon Structure
- Installed rails and grates at Luce Bayous Siphon
- Installed reinforced concrete box and drainage structures
- Continued installing coping at Retaining Walls 8, 9, 10, 13, 24 and 26
- Established embankment utilizing day and night shifts
- Backfilled and installed MSE panels at Retaining Walls 14, 15, 16, 17, 18 and 19
- Delivered, tied rebar cages for drilled shafts, formed, poured and stripped drilled shaft extensions and drilled shaft foundations for Bridge 231
- Formed and poured bent caps as well as cast-in-place walls for Bridge 231
- Continued to deliver, tie rebar cages for drilled shafts and columns for Bridge 236
- Formed, poured and stripped drilled shaft extensions, columns and caps for Bridge 236

Segment 2:

Activities in H2 included:

- Continued PI mapping at the Stoesser borrow pit site

- Extended haul road and stabilized it to create access for Bridge 254 activities
- Stripped topsoil for detention ponds
- Placed embankment in lifts
- Completed reinforced concrete pipe installation at FM 686
- Excavated ditches, installed reinforced concrete box equalizers and finished installation of drain lines
- Installed temporary drainage across CR 621
- Installed MSE panels and backfilled at Retaining Walls 6,7,8, 9, 10 and 11,
- Levelled pad construction at Retaining Walls 10 and 11
- Progressed ground improvement for Retaining Wall 11
- Completed drilled shafts at Abutment 2 and placed drilled shaft extensions at Abutments 1 and 2 for Bridge 252
- Constructed drilled shafts at bent 3, 4 and 5 for Bridge 254

Activities in I1A included:

- Continued construction of embankment and hauling from borrow pit
- Progressed stripping topsoil, clearing and grubbing operations
- Completed drainage pipe installation and backfill for restrictor pond #1
- Completed installation of headwalls for culverts 9, 10, 11 and 12
- Levelled pad construction at Retaining Walls 10, 11, 12 and 16
- Completed backfilling MSE panel installation at Retaining Walls 13, 14, 15 and 17
- Completed ground improvements for Retaining Walls 7, 8, 9, 10, 11 and 12
- Started leveling pad construction at Retaining Walls 7, 8 and 9
- Placed beams and completed bridge deck and approach slabs at Bridge 4A
- Completed columns construction for bent 2 through 5 of Bridge 7A
- Completed footings for bents 9 through 12 of Bridge 7A and for bent 2 of Bridge 7B
- Worked on abutment 6 at Bridge 5
- Continued approach slabs construction at Bridge 6
- Worked on bent 5 drilled shafts at Bridge 8
- Drilled and poured drilled Shafts at bents 3 through 6 and drilled shaft extensions at abutment 1 of Bridge 254

Segment 3:

Activities in I1B included:

- Excavated ground to top of shaft to initiate bent 2 drilled shaft construction at Bridge 12
- Completed installation of culvert at CWA Canal

Activities in I2A1 included:

- Continued to place embankment and lime stabilize
- Poured and stripped footings for bents 2 and 3 at Bridge 15



- Set column formwork and poured concrete for bent 2 at Bridge 15
- Formed deck overhangs at spans 4 and 5 of Bridge 15
- Placed decks unit and deck rebar for spans 13, 14, 15, 16, 17, 18 and 19 at Bridge 15
- Worked on backfill, foundation improvements leveling pad installation and MSE panel installation at Retaining Walls 4, 5 and 6
- Completed demolition of existing roadway near Kilgore

Activities in I2A2 included:

- Constructed reinforced concrete box west of Fisher
- Excavated for foundation improvements for Retaining Wall 1
- Worked on leveling pad installation for Retaining Wall 2

Activities in I2B1 included:

- Continued embankment construction, including construction of cement treated base and remixing lime
- Carried out slope grading for Retaining Walls 1, 3, 4 and 6
- Completed leveling up and placing coping at Retaining Walls 2 and 5
- Formed, poured and stripped cast-in-place wall for abutments 1 and 4 of Bridge 20
- Set concrete girders, installed deck panels and formed overhangs for spans 1, 2 and 3 of Bridge 20
- Switched traffic on eastbound frontage road to advance to next phase of work
- Initiated concrete paving operation on eastbound frontage road and main lanes

Activities in I2B2 included:

- Prepared for lime operations at frontage road
- Established embankment east of Beach Rd Business 146
- Backfilled and installed MSE panels Retaining Walls 22, 23, 24 and 25
- Continued leveling pad, underdrains, backfilling and MSE panel installation at Retaining Walls 26, 27 and 28
- Continued concrete paving on eastbound and westbound mainlanes at Gooselake

## 4.0 Substantial Completion

According to the most recent progress schedule submitted by the DB Contractor in this reporting period, Projected Substantial Completion of H and I Project is indicated to occur on May 12, 2022, which is outside the 1,693 days that are available to the DB Contractor after accounting for approved change orders. Therefore, the DB Contractor is forecasting a delay of 74 days as of end of quarter included in this report. Project completion through Final Acceptance is projected to be 120 days after the Projected Substantial Completion on September 9, 2022. The DB Contractor has made submittals including several revisions and has

been in discussions with TxDOT regarding Time Impact Analysis in compliance with requirements of the DBA to account for impacts of Tropical Storm Imelda. Upon review and approval of the Time Impact Analysis, the DB Contractor will be credited days due to the impacts to its operations. See Section 5 of this report for additional information on schedule impacts from Tropical Storm Imelda.

DB Contractor has an interim milestone requirement to complete work in Toll Zones 180 days prior to Substantial Completion Deadline and handover to Toll System Integrator. According to the latest schedule from the DB Contractor sufficient progress will be made in Toll Zones to complete the handover to Toll System Integrator on October 13, 2021. Pending approval of the additional days due to Tropical Storm Imelda this target date is within the requirements of the DBA. TxDOT continues to monitor the schedule for turning over the Toll Zones to the System Integrator.

## 5.0 Material Problems Encountered

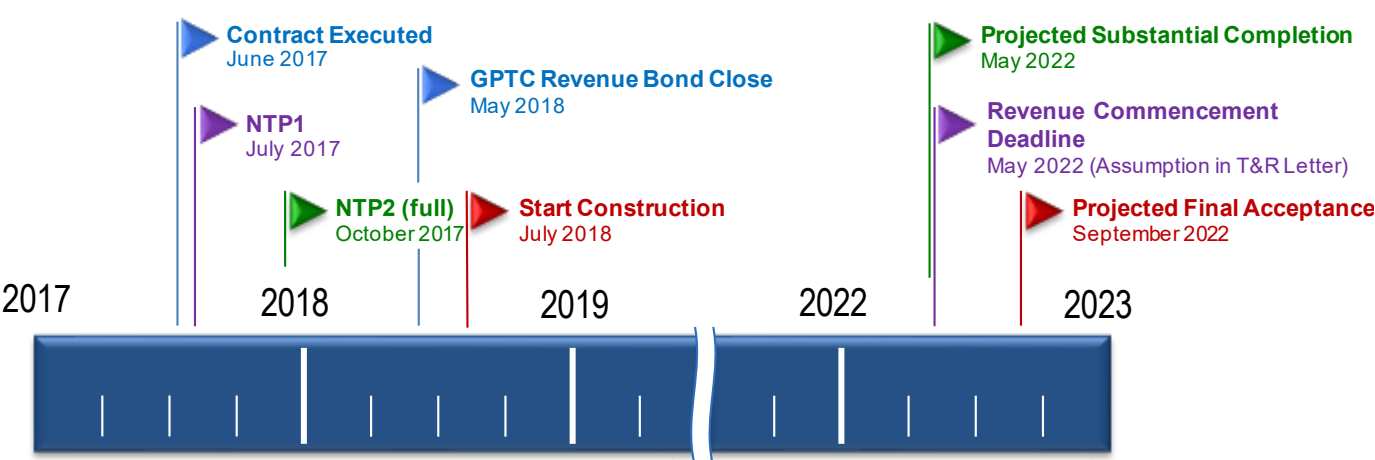
On September 19, 2019, Texas Governor Greg Abbott declared a state of disaster for the counties impacted by Tropical Storm Imelda. Each of the four counties: Chambers, Harris, Liberty and Montgomery traversed by the project were covered by the Governor's declaration. Work production was affected due to flooding within the project right of way, laydown areas and at borrow sites. In general, this included: damage to fixtures, materials, supplies, tools, machinery and equipment; damage to in-progress works such as walkways, signs, formwork, falsework, site-access, etc.; lost time and cost due to restoration activities and rework; and debris removal, clean-up and restoration of work site to pre-tropical storm state.

The DB Contractor made a detailed submittal of Time Impact Analysis from Tropical Storm Imelda, including several revisions following reviews by TxDOT to be compliant with the requirements of the DBA. These submittals document schedule impact of Tropical Storm Imelda on construction work. It details activities that had to be performed to bring progress back to the pre-storm stage as well as carry-out additional tasks necessitated by the event. Upon final review and approval of the Time Impact Analysis, it is expected that the DB Contractor will be credited a reasonable amount of days that can be proven to have impacted the critical path, but this is currently expected to be less than the 74 days requested by the DB Contractor.

As of February 20, 2020, reporting period included with the latest draw submittal made by the DB Contractor (in this reporting period), actual schedule percent complete was 39% (as measured by the Primavera performance based cost loaded schedule submittal). This is behind the Planned Schedule Percent Complete of 49%. Similarly, earned value reported by the DB Contractor and included in Table 2.3 of this report shows that overall progress is behind what was originally estimated and included in the form of the maximum payment curve. Earned value in the most recent three quarters had trailed off as the DB Contractor appeared to be challenged by some staffing issues on earthwork to meet full planned production efforts and subsequently was further impacted by the effects of Tropical Storm Imelda. While additional days provided to the DB Contractor to account for impacts from Tropical Storm Imelda provides some relief, this overall trend is expected to continue.

Projected Substantial Completion as of this reporting period is expected to occur on May 12, 2022. The DB Contractor continues to be monitored closely and will be required to submit a recovery schedule should any activity on the Critical Path be delayed greater of 30-days or a number of days equal to 5% of the days remaining until a completion deadline after considering any additional days added to the contract. Upon final approval of the Tropical Storm Imelda related Time Impact Analysis, any additional approved days added to the contract will be reflected in a revised baseline schedule.

Commencement of toll revenues is anticipated to occur at Projected Substantial Completion, which is currently estimated to be May 12, 2022, as described above, and is before the Revenue Commencement date included in the CDM Smith traffic and revenue assumptions. CDM Smith’s 2018 T&R Bring Down Letter assumes that Segments H, I-1 and I-2B will be open to tolled traffic on May 22, 2022.



Note:  
Future dates are subject to change

**Figure 5.1: H and I Project Milestones**

The DB Contractor brought forward the escalation in domestic steel prices affecting reinforcement steel and guard rail. The DB Contractor had made a request for partnering discussion in accordance with the provisions of the DBA to discuss whether there is any entitlement to a relief through change order. These discussions were held and TxDOT’s position was that DB Contractor is not entitled to such relief under the DBA. The DB Contractor has escalated this issue to the Dispute Review Board (DRB) as allowed under the DBA. The Dispute Resolution Process is currently under process and is following the steps and schedule that have been agreed with all the entities including the DRB members, with initial hearing scheduled for April 22, 2020.

Several other requests for partnering has been made by the DB Contractor where the DB Contractor initiated changes are in initial review or have been rejected by TxDOT. These include

changes associated with toll charges from toll road usage and Bridge span-length requirement at US90 pertaining to UPRR right of way.

## 6.0 Delivery Status of Equipment

No issues with equipment deliveries from the DB Contractor or the Toll System Integrator are currently anticipated. DB Contractor will procure the ITS and Traffic Signal equipment which is routine for such project.

TransCore is responsible for installing Tolling Equipment such as transponder readers and cameras, and has done so on other projects for TxDOT on a statewide basis.

## 7.0 Change Orders

### 7.1 Approved Change Orders

As of the period covered in this progress report, one change order for extending the Kingwood Drive Bridge to accommodate future U-turns under the bridge was approved:

#### Change Order #4

- Kingwood Drive Overpass Ramps & U-turn
  - Amount = \$6,522,000
  - Days = 0

Appendix B provides the overall list of approved change orders. With the approval of change orders as listed in Appendix B, the total construction cost for the DBA has increased from \$894,701,621.55 to 906,946,486.88 and total days have increased from 1,674 to 1,693.

### 7.2 Pending Change Orders

A change order comprising revisions to Bridge #8 configuration over Rail Yard/Speer Property is pending approval. TxDOT issued a directive letter to DB Contractor to describe the scope of work and to lock the already negotiated lump sum change order amount. The bridge modifications in this pending change order include longer structural steel spans and a longer bridge to accommodate savings in right of way costs that avoid impacts to existing and proposed rail yard operations just north of Mont Belvieu which serve the petrochemical industries in that area. Anticipated value of this pending change order is as follows:

#### Pending Change Order #3

- Bridge Over Rail Yard/Speer Property
  - Amount = \$17,600,000
  - Days = 0

## 8.0 Right of Way Acquisition

TxDOT carried out early right of way acquisition activities prior to issuance of NTP2. With the issuance of the NTP2, the DB Contractor is made responsible for all right of way acquisitions. Based on progress reporting from the DB Contractor, to date status of acquisitions is as follows:

**Table 8.1: Progress on Right of Way Acquisitions**

ROW Status	
Estimated Total Number of Parcels <sup>1</sup>	235
Surveys Submitted for TxDOT Approval <sup>2</sup>	235
Right-of-Entry – Signed or verbal <sup>3</sup>	217
Parcels in Possession and cleared for construction <sup>4</sup>	194

**Notes:**

1. Estimated total number of parcels represents most recent estimate of total acquisitions required based on the status of design development.
2. Surveys submitted for TxDOT approval represents approved surveys and those pending approvals as of the report period. Surveys previously submitted but requiring modifications are not included in the count until resubmitted.
3. Right-of-Entry figures represent active verbal or signed right of entries excluding those that may have expired.
4. As of reporting period, 194 out of the estimated total parcels to be acquired, or 82% are cleared for construction.

The DB Contractor right of way acquisition activities are on track as indicated by the earned value reporting and actual percent complete discussed in Sections 2 and 5 respectively. A sufficient number of parcels have been cleared for construction to progress.

## 9.0 Discussion of Other Matters

On March 13, 2020, the President of the United States declared the outbreak of COVID-19 a national emergency and the Texas Governor declared it an imminent threat of disaster for all counties (including Chambers, Harris, Liberty and Montgomery) in Texas. Since these declarations were made after the period covered in this report, any impacts from the COVID-19 outbreak on the progress of the H and I Project will be addressed in the next Quarterly Construction Progress Report.

## APPENDIX A – Construction Photos for the Quarter



**Figure A.1: Nighttime Beam Placement at Bridge 206 in Segment H1A**



**Figure A.2: Beam Placement at Bridge 204 in Segment H1A**



**Figure A.3: Setting Deck Panels for Span 3 at Bridge 203 in Segment H1A**



**Figure A.4: Placement of MSE Panels at Retaining Walls 17, 18 & 19 in Segment H1C**



**Figure A.5: Construction at Retaining Wall 13 in Segment H1C**



**Figure A.6: Embankment Construction in Segment H2**



**Figure A.7: RCP Installation at FM686 Crossing in Segment H2**



**Figure A.8: MSE Construction at Bridge 250 in Segment H2**



**Figure A.9: Concrete Pour at Bridge 7A Bent in Segment I1A**





**Figure A.10: Beam Placement at Bridge 4A in Segment I1A**



**Figure A.11: Bent Cap Construction at Bridge 15 in Segment I2A1**



**Figure A.12: Deck Rebar installation at Bridge 15 in Segment I2A1**



**Figure A.13: Lime Stabilized Embankment Construction at Ramp 16 in Segment I2A1**



**Figure A.14: Concrete Paving at Frontage Road in Segment I2B1**



**Figure A.15: MSE Panel Placement for Retaining Wall 22 in Segment I2B2**

## APPENDIX B – Change Orders List

Change Order No.	Description	Status	Date Executed	Time Impact (days)	Cost Impact (\$)
1	Hurricane Harvey Impact & NTP2 Delay	Executed	11/20/18	19	-
2a	Castle Hill CH Partners Utility Casings	Executed	9/18/18	0	849,097
2b	Remove RR Track from UPRR Structures	Executed	9/18/18	0	73,941
4	CWA Siphon at Luce Bayou	Executed	5/3/19	0	4,799,827
5	Kingwood Drive Overpass Ramps & U-turn	Executed	2/9/20	0	6,522,000
Subtotal Executed Change Orders				19	12,244,865
3	Bridge Over Rail Yard/Speer Property	Pending	NA	0	17,600,000
Subtotal Pending Change Orders				0	17,600,000
Total Executed and Pending Change Orders				19	29,844,865

Figures shown may not add to totals due to rounding.

**Note:**

With the approval of change orders 1, 2a, 2b, 4 and 5 the total construction cost for the DBA has increased by \$12,244,865.33 from \$894,701,621.55 to \$906,946,486.88 and total days have increased from 1,674 to 1,693.

## APPENDIX C – Acronyms and Abbreviations

ATC	Alternate Technical Concept
AUA	Abbreviated Utility Agreement
CPM	Critical Path Method
DB	Design Build
DBA	Design Build Agreement
ED	Eminent Domain
ESA	Environmental Site Assessment
FHWA	Federal Highway Administration
FM	Farm to Market Road
GEC	General Engineering Consultant
GPI	Grand Parkway Infrastructure, LLC (DB Contractor)
GPTC	Grand Parkway Transportation Corporation
IH	Interstate Highway
IP	Individual Permit
ITS	Intelligent Transportation Systems
MSE	Mechanically Stabilized Embankment
NCR	Non-Conformance Report
NEPA	National Environmental Policy Act
NTP	Notice to Proceed
NWP	Nationwide Permit
PUA	Possession and Use Agreement
PUAA	Project Utility Adjustment Agreement
RFC	Released for Construction
RFI	Request for Information
ROE	Right of Entry
ROW	Right of Way
SH	State Highway
SI	Systems Integrator
SW3P	Storm Water Pollution Prevention Plan
TTC	Texas Transportation Commission
UPRR	Union Pacific Railroad
US	United States Highway
USCG	United States Coast Guard
USACE	United States Army Corps of Engineers
T&R	Toll and Revenue
TxDOT	Texas Department of Transportation
UAAA	Utility Adjustment Agreement Amendment

GRAND PARKWAY TRANSPORTATION CORPORATION  
UNITED STATES DEPARTMENT OF TRANSPORTATION  
TIFIA LOAN

**Authorized Representative Certificate**

I, Benjamin Asher, Secretary/Treasurer of the Grand Parkway Transportation Corporation, hereby certify that the Quarterly Construction Progress Report to Grand Parkway Transportation Corporation has been prepared in connection with Section 23 of its TIFIA Loan Agreement with the United States Department of Transportation dated February 21, 2019.

The undersigned has executed this Authorized Representative Certificate as of the date set forth below.

DocuSigned by:

*Benjamin H. Asher*

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Mr. Benjamin H. Asher  
Secretary/Treasurer  
Grand Parkway Transportation Corporation  
Borrower's Authorized Representative

4/17/2020

Date